

2014
CORDUROY
ENDURO

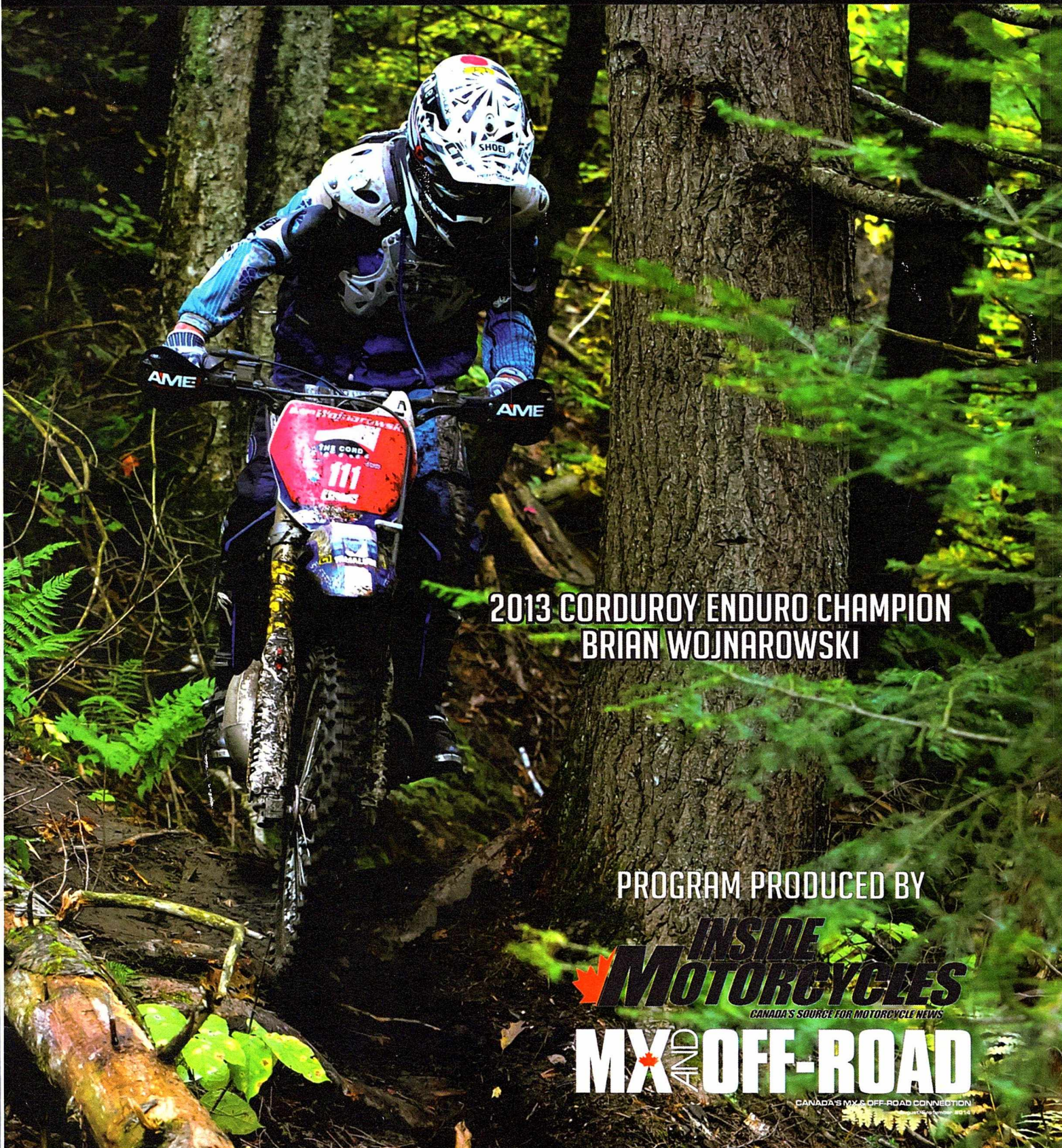


CARRIER CENTERS
MOTORCYCLES



OFFICIAL EVENT PROGRAM

PRESENTED BY CARRIER CENTERS MOTORCYCLES



2013 CORDUROY ENDURO CHAMPION
BRIAN WOJNAROWSKI

PROGRAM PRODUCED BY

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KIMPEX

RIDER BIOS



Aaron Kopp

Age: 40
Hometown: Gibsonia, PA
Bike: 2012 Yamaha, YZ250
Class: Pro
Event Number: 114
Sponsors: AXO, Boyesen, Twin Air
About: Aaron is a three-time US ISDE rider with two appearances on the trophy team, multi-time national motocross qualifier, three-time ECEA Overall Champion and the 50th anniversary Corduroy overall winner. He is married with two children and works as an application developer in Pittsburgh and spends his free time enjoying cross country skiing, mountain biking and trail running.



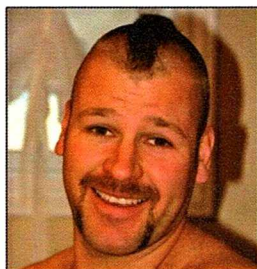
Aaron Wilkins

Age: 29
Hometown: Minden, ON
Bike: 2005 KTM, EXC300
Class: PRO
Event Number: 118
About: Having ridden since the age of three, Aaron enjoys the challenge that the Corduroy poses and as always his goal is just to finish. The Cord is a local race for the Minden resident, so he is familiar with the terrain.



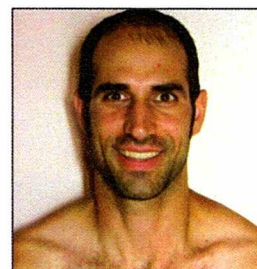
Adam Millson

Age: 33
Hometown: Pontypool, ON
Bike: 2013 Yamaha, YZ250
Class: Pro
Event Number: 117
Sponsors: Machine Racing, Funland
About: Adam started riding at the age of 12, and likes to spend his time hanging out with friends and family, riding and playing hockey in the winter. He is married with three children and is the older brother of Expert rider Andrew.



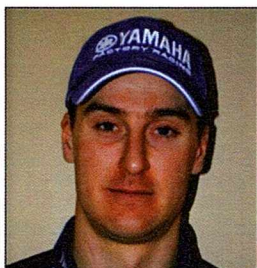
Alexandre Legault

Age: 33
Hometown: Ferme-Neuve, QC
Bike: 2013 Yamaha, YZ250
Class: Pro
Event Number: 113
Sponsors: Sale Suspension, Motovan, Sport Motorisé Honda, Mont-Laurier Sports
About: Alexandre started riding at the age of 15 on a XL 200R and started racing at 21 and turned Pro at 27. He is currently in Second position in the 2014 Quebec Enduro Championship (FMSQ) and his goal is to win a Pro championship.



Brian Wojnarowski

Age: 35
Hometown: Pontypool, ON
Bike: 2014 Yamaha, YZ250
Class: Pro
Event Number: 116
Sponsors: Yamaha, Machine Racing, LGS, AME grips, Orthoflex, Podium, PerformX, Mongoose machine, Rekluse, Atlas, GYTR.
About: Brian really enjoyed the Cord in 2013 and took his first Corduroy overall win and has been looking forward to this race all year.



Bryan Marshall

Age: 37
Hometown: Utopia, ON
Bike: 2013 Honda, CRF250R
Class: Pro
Event Number: 115
Sponsors: Machine Racing, Landing Gear Suspension, Swampy's Racing Supplies



Connor Brogan

Age: 19
Hometown: Palgrave, ON
Bike: 2013 Husaberg, TE300
Class: Pro
Event Number: 109
Sponsors: LGS, Machine Racing, 100%, Atlas Brace
About: Connor has been racing the Offroad Ontario series' since it started and this is his first year riding pro and he is looking forward to riding all three days of the Corduroy this year.



Ken Beach

Age: 22
Hometown: Perth, ON
Bike: 2012 KTM, 350 SX-F
Class: Pro
Event Number: 111
Sponsors: Woody's Cycles, KTM Canada, Stadium Suspension, LRP, Clockwork KTM, Woodvale Racing, Motovan.
About: Ken is just 22 years old and is a Tech at Woody's Cycles in Perth, ON. He has been racing off-road for 10 years and was Canadian XC champion in my class for 2012 and a 2013 ISDE medalist. This will be his fifth Cord and his first time doing all three days! The Cord is his type of terrain and he enjoys the rocky and nasty trails that has become famous for.



Thierry Lacombe

Age: 46
Hometown: Beloeil, QC
Bike: 2009 Husqvarna, WR300
Class: Pro
Event Number: 110
Sponsors: Stadium Suspension



Zachary Lewis

Age: 18
Hometown: Pontypool, ON
Bike: 2014 Yamaha, YZ250
Class: Pro
Event Number: 112
Sponsors: Yamaha Motor Canada, Machine Racing, Hudson Henderson Insurance, Scott Gear, AME, Grandpa
About: Zach loves to learn and is always up for a tough challenge. In his words, "Racing is my life and if it were easy everyone would do it."



Andrew Millson

Age: 30
Hometown: Pontypool, ON
Sponsors: 2011 Yamaha, YZF 250
Class: Expert
Event Number: 217
About: Andrew is from the small town of Pontypool, ON which has brought up some fast enduro riders over the years. He has had the pleasure to ride with some of them for the past twenty years, but it wasn't till 2012 when he raced my first Cord and took the top step of the podium in the Novice class and again in 2013 in Intermediate.



Barry Armstrong

Age: 34
Hometown: Milton, ON
Bike: 2014 Husaberg, FE350
Class: Expert
Event Number: 207
Sponsors: I wish I had some....
About: Barry bought his first dirt-bike in 2004, a KTM 640E dual-sport, and have been loving off-road riding ever since. He enjoys challenging, technical riding, and wishes he could ride better.



Erik de Cotret

Age: 38
Hometown: Cantley, QC
Bike: 2013 KTM, 300 XC-W
Class: Expert
Event Number: 206



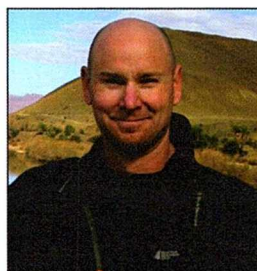
James McNally

Age: 32
Hometown: Toronto, ON
Bike: 2013 KTM, 250 XC-W
Class: Expert
Event Number: 211
Sponsors: The Fouled Plugs
About: James started life on two wheels at the age of five and returned to riding four years ago after a hiatus for school, work and starting his family. This will be his forth Corduroy Enduro and first in the Expert class.



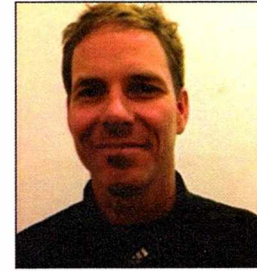
Jeremy Quinn

Age: 35
Hometown: Mississauga, ON
Bike: 2010 KTM, 250 XC
Class: Expert
Event Number: 218
About: This is Jeremy's fourth season racing and he is determined to learn to ride faster. Having started in his 30s it's a race against time!



Brent Phifer

Age: 41
Hometown: Carleton Place, ON
Bike: 2013 KTM, 200 XC-W
Class: Vet A
Event Number: 318
Sponsors: :My loving wife
About: Brent has been riding for 20 plus years and was the 2013 Vet B OO XC champion, 2013 Vet B OO grand champion, 2013 corduroy finish-dnf, hoored out on Sunday. His best Corduroy finish came in 2012 where he got third in Vet B. His goal for the 2014 Corduroy is a top ten finish in the stacked Vet A class.



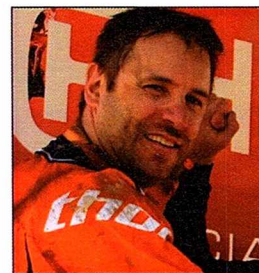
Craig Kennedy

Age: 54
Hometown: Vienna, ON
Bike: 2014 Yamaha, YZ250
Class: Vet A
Event Number: 209
Sponsors: Machine Racing



Gilles Bussiere

Age: 50
Hometown: St-Bruno, QC
Bike: 2010 KTM, 250XC-W
Class: Vet A
Event Number: 305
Sponsors: Sly Racing
About: This will be Gilles' 20th Corduroy Enduro.



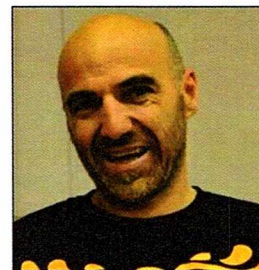
Matthew Spigelmyer

Age: 50
Hometown: Spring Mills, PA
Bike: 2007 Honda, CR250R
Class: Vet A
Event Number: 216
Sponsors: Matto Cycle



Mike Marcoux

Age: 48
Hometown: St Jean Baptiste, QC
Bike: 2013 Beta 250RR
Class: Vet A
Event Number: 210
About: Mike has plenty of riding experience including three times at the Dacre Challenge, twice in the Orange Crush QC Rally, four times at the Alligator Enduro in Florida, and many other enduros and GNCCs in US. He is a self employed mechanic for motorcycles and cars. He rides a 250 Beta for racing a KTM 530 for rally and a KTM 950 Adventure for long trips.



Paul Andratis

Age: 50
Hometown: Simcoe, ON
Bike: 2009 Husqvarna, WR300
Class: Vet A
Event Number: 203
Sponsors: Lang's Off-Road
About: Paul entered his first Enduro in 1982 at age of 18 and took his first win at the Beagle Bash Enduro in 1984. Many more overall wins followed on the way to three Ontario and National Enduro Championships in the Masters (Pro) Class. He was the overall winner of the Corduroy Enduro in 1987 on the Husqvarna 430 Automatic and has earned three ISDE medals.



Rome Haloftis

Age: 47
Hometown: Mississauga, ON
Bike: 2014 Sherco, SEF-R300
Class: Vet A
Event Number: 202
About: Rome is the Event Coordinator for the OFTR, OO Vet X Grand Champion 2012, finished the Red Bull Romanian's 2012, Iron Manned the Baja 1000 in 2006, and this will be his 12th Corduroy Enduro.



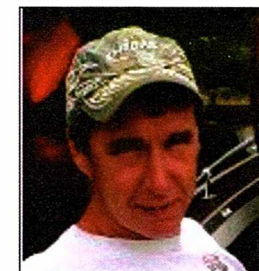
Steve Oomen

Age: 42
Hometown: Palgrave, ON
Bike: 2006 Honda, CR250
Class: Vet A
Event Number: 102
Sponsors: Steve Oomen Contracting



Tim Carney

Age: 45
Hometown: Caledon, ON
Bike: 2012 Husaberg, TE300
Class: Vet A
Event Number: 208
Sponsors: Peter Carney Plumbing & Heating Ltd., Jill Carney
About: Tim is married with four children and runs a plumbing and heating business and have been riding most of my life. He raced CMA motocross in the late 80s and eventually stopped riding altogether until buying a XR650R in 2008 with the intention of just trail riding. A friend said, "Let's try a club cross country race at Burnt River," after which I was hooked on racing again.



A BRIEF LOOK AT THE CORDUROY ENDURO



Few events in Canada, and even the world, share the kind of history that the Corduroy Enduro proudly owns. When the founders of the Corduroy Enduro first mapped out the route they started at the Gold Rock Lodge, near Cobocok, ON, they never thought that it would turn into the event that it has. It would have been hard to imagine, in 1953, that the Cord would still be running in the year 2014, and that it would have winners from as far away as Australia and New Zealand. This year marks the 61st running of what is now the Gas Gas, Ossa Corduroy Three Day Enduro Presented by Carrier Centers Motorcycles and counts as three rounds in the Sherco Canadian National Enduro Championship.

The inspiration for the Corduroy Enduro came from a fateful trip to the Jack Pine Enduro by Don Charters and Ron Jackson. After their trip they came back with the idea of having a Canadian two day endurance event and after some hard work, the Corduroy was born. The early years started off small with only 14 riders entering. By the third year 28 riders tried their hand at the Corduroy, but the table had been set for how challenging the Cord would be and history was set in motion with Bill Sharpless taking the win.

When Sharpless captured his first Corduroy win he set events in motion that would see his family name go on to secure the most wins of this arduous race. Between Sharpless and his son and current Corduroy grand poobah, Blair, the Sharpless name owns 10 overall titles. The Corduroy has proven to be an infectious event, with many participants returning year after year to punish themselves in the Haliburton Highlands.

Word quickly spread throughout Ontario and into the US with plenty of Americans eager to test their mettle against the Canadians and, to date, the Americans have the slight edge in overall Cord victories. In the early days and still to this day, the camaraderie and fun that was found around the camp was as important as the race itself. Why else would we put ourselves through the punishment that is the Cord? That is not to say that you will not find some intense competition, but you will also find direct competitors helping each other through some of the tougher sections.

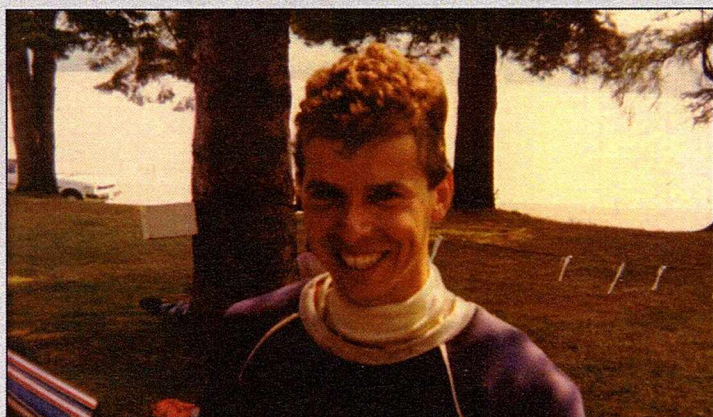
A fair share of local heroes have taken home the famous Cord Log, starting with Basil Jackson in 1953 and most recently with Brian Wojnarowski in 2013. But the Cord has attracted more than enough big US and international riders and that includes the Penton family of John, Jeff and Jack, not to mention one of the best enduro riders in the world in Chris Birch of New Zealand.

A few short years after the founding of the Corduroy Enduro, the

Canadian Motorcycle Association deemed it worthy of being a National Championship. To reach that level in less than five years attests to the special nature of the race. While it may not technically be a National Championship anymore, it is still certainly a special race and the challenge that it poses make it worthy of any rider across Canada.

What makes the early days of the Cord even more impressive is that the brave souls that attempted to cross the Haliburton Highlands did so aboard heavy street motorcycles that were often ridden to the start of the race and then ridden home again after. This speaks volumes about the men that first started the Cord and the many that signed up to put themselves through the wringer. For several years the winners were on big Triumphs, Harleys, Ariels and Nortons to name a few. None of those machines were under 500 cc and you better believe that they were a fair bit heavier than the current enduro bikes. It took a special kind of person to be willing to take their daily driver that weighs more than 400 lb and send it through water and mud all in the name of fun.

That spirit can still be seen at the base camp of the Corduroy Enduro, now located in Gooderham. Well over 100 men and women willingly sign up to put themselves and their expensive motorcycles through a torture test and do it with a smile on their face. What is it that draws riders to this race year after year? Is it the challenge?



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A BRIEF LOOK AT THE CORDUROY ENDURO



Yes. Is it the fun? Yes. Is it the camaraderie? Yes. It is all those reasons and many more.

Enduro riders have a great sense of community and at no other race is that more evident than the Corduroy Enduro. That is part of the advantage of racing against the clock, you can race your own race and still have a drink with your closest rival at the end of the day with no hard feelings. One long-running tradition that helps foster that sense of community is what started out as the Saturday night turkey dinner at the Gold Rock Lodge. This gives the riders an opportunity to sit around and tell lies about how they rode and how easy the day was, all the while listening to the others do the same thing. Plenty of lifelong friendships have been formed at the Saturday dinner table and many more will be formed in the years to come.

Another unique feature of the Cord is that just when you thought you had enough, there is another day left to test man and machine.

You see, this race not only challenges you physically and mentally, but it also forces you to be smart with your bike. In order to capture a famous Corduroy Log you must keep your bike in one piece with only a minimal amount of maintenance before locking it up in the impound until the following morning.

The second day is always a test because you are sore from Saturday's adventure and you have to do it all over again, with likely too little sleep. If you are fortunate enough to make it to the end of Sunday you will have a sense of accomplishment that you may not expect. After all, it is just a dirt bike race. For many people, this will be the hardest thing that they ever do and even fewer will be lucky enough to come away with a log.

You see, the Cord Log is not just another trophy, it is a symbol of endurance, character, luck and companionship. Those are the things that encapsulate the Corduroy Enduro and that is why the log will hold a special place in your trophy case.

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A BRIEF LOOK AT THE CORDUROY ENDURO



Brian Wojnarowski
holding the 2013 Log.

CORDUROY WINNERS

1953	Basil Jackson	Toronto, ON	500 Triumph	1983	Blair Sharpless	Toronto, ON	250 Can-Am
1954	Gerald Roberts	Toronto, ON	750 Harley-Davidson	1984	Blair Sharpless	Toronto, ON	250 Can-Am
1955	Bill Sharpless	Toronto, ON	650 Ariel	1985	Brian Mull	Concord, MI	250 Husqvarna
1956	Sheldon Smith	Peterborough, ON	500 AJS	1986	Drew Smith	Great Meadows, NJ	250 Honda
1957	Leroy Hartman	Plymouth, MI	600 Norton	1987	Paul Andratis	Delhi, ON	430 Husqvarna
1958	Clarence Wise	Zanesville, OH	650 Triumph	1988	Blair Sharpless	Toronto, ON	250 Suzuki
1959	John Penton	Lorain, OH	175 NSU	1989	Blair Sharpless	Toronto, ON	250 Suzuki
1960	John Penton	Lorain, OH	175 NSU	1990	Blair Sharpless	Toronto, ON	250 Suzuki
1961	John Penton	Lorain, OH	250 BMW	1991	Blair Sharpless	Toronto, ON	250 Suzuki
1962	Norm Smith	Brecksville, OH	500 Triumph	1992	Kevin Hines	Weston, MA	350 Husqvarna
1963	Sal Scirpo	Hartford, CT	250 Harley-Davidson	1993	Blair Sharpless	Brooklin, ON	250 Suzuki
1964	John Penton	Lorain, OH	250 BMW	1994	Matt Spigelmyer	State College, PA	250 Suzuki
1965	John Penton	Lorain, OH	250 BMW	1995	Craig Kennedy	Ayler, ON	250 Honda
1966	Bill Sharpless	Toronto, ON	250 Bultaco	1996	Kevin Hines	E. Wareham, MA	150 CRE/Honda
1967	Bill Sharpless	Toronto, ON	250 Bultaco	1997	Matt Spigelmyer	Furnace, PA	250 KTM
1968	Leroy Winters	Fort Smith, AR	125 Penton	1998	Kevin Hines	E. Wareham, MA	150 CRE/Honda
1969	Event Cancelled			1999	Matt Spigelmyer	Furnace, PA	250 KTM
1970	Jeff Penton	Lorain, OH	125 Penton	2000	Allan Lachappelle	Bolton Centre, QC	250 Kawasaki
1971	Jeff Penton	Lorain, OH	125 Penton	2001	Glenn Buchanan	Toronto, ON	250 Yamaha
1972	Dave Hulse	Bay Shore, NY		2002	Glenn Buchanan	Toronto, ON	250 Yamaha
1973	Tom Penton	Lorain, OH	125 Penton	2003	Aaron Kopp	Center Hall, PA	250 KTM
1974	Jeff Smith	Valcourt, QC	175 Can-Am	2004	Leonard Keene	Michigan	250 KTM
1975	Tom Penton	Lorain, OH	250 Penton	2005	Patrick Beaule	St Julie, QC	400 KTM
1976	Max Markowitz	Putman, CT	400 Penton	2006	Patrick Beaule	St Julie, QC	250 KTM
1977	Max Markowitz	Putman, CT	250 Penton	2007	Jake Stapleton	Australia	250 Yamaha
1978	John Cooper	Columbus, NJ	175 KTM	2008	Robbie Jenks	New Lexington, OH	450 Husaberg
1979	John Cooper	Columbus, NJ	400 Maico	2009	Patrick Beaule	St Julie, QC	450 KTM
1980	Tom Krehbiel	Vienna, VA	250 KTM	2010	Patrick Beaule	St Julie, QC	450 KTM
1981	Jamie Stevens	Winona, ON	175 Yamaha	2011	Bobby Prochnau	Savona, BC	450 KTM
1982	Ross Lennox	Orangeville, ON	400 Can-Am	2012	Chris Birch	New Zealand	500 KTM
				2013	Brian Wojnarowski	Pontypool, ON	250 Yamaha



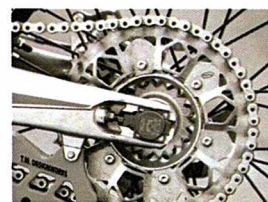
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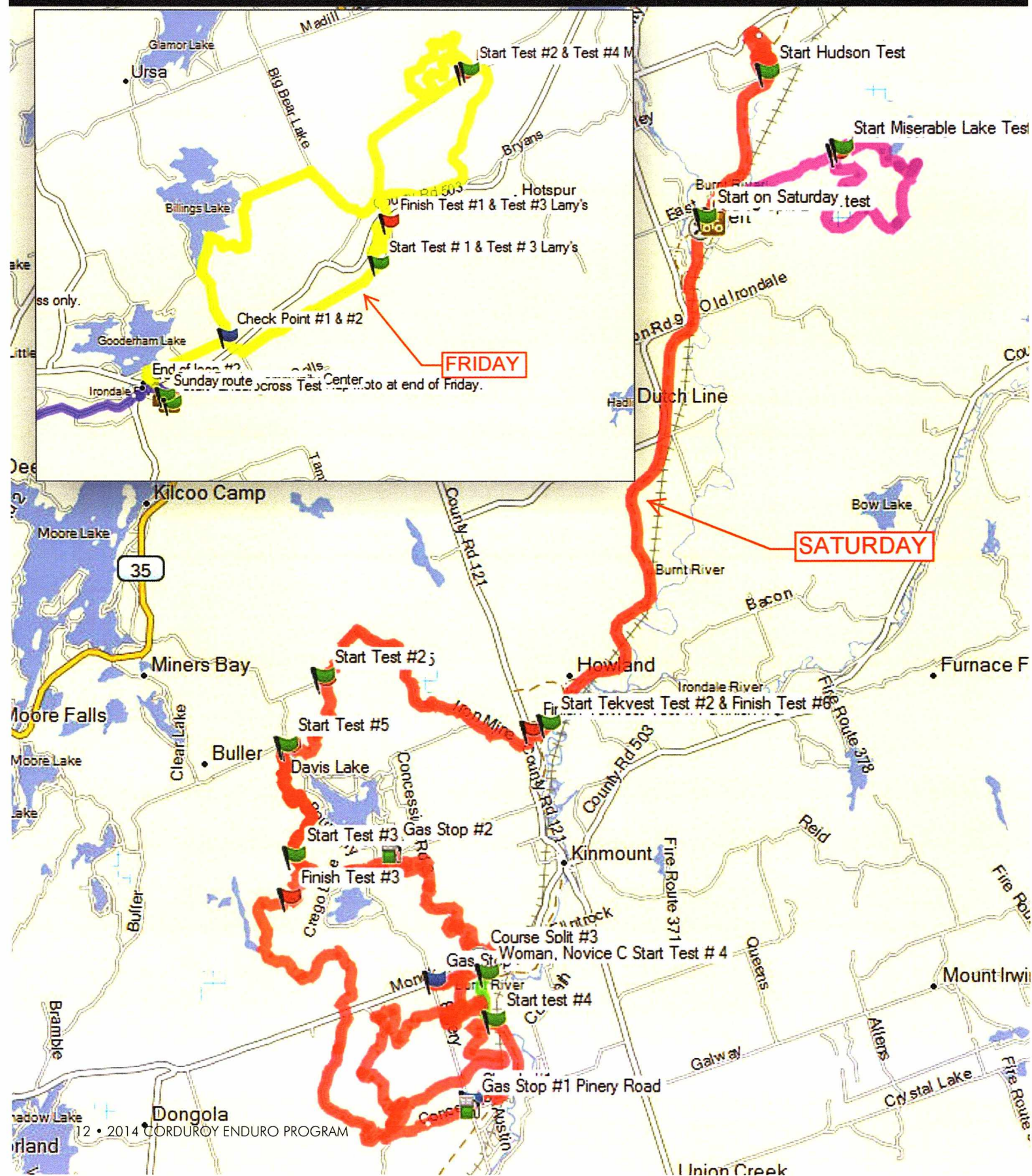
2014 CORDUROY ENDURO



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ROUTE MAP FRIDAY/SATURDAY





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EVENT SCHEDULE - 61st CORDUROY 2014

Friday

(all at the Gooderham Community Centre)

9:00 AM Checkers meeting
9:00-11:00 AM Sign-in and sound test for riders riding all 3 days (Pro, Expert, Vet A)
10:00 AM Riders Meeting (Pro, Expert, Vet A)
11:00 AM The 61st Corduroy Enduro begins
1:15 PM First rider is due into the work area for bike maintenance
3:00 PM Endurocross Test
5:00-9:00 PM Sign-in and sound test for ALL CLASSES. T-shirt and meal ticket sales open.
7:00 PM Check Crew Meeting
8:00 PM Riders Meeting for ALL CLASSES Saturday

Saturday

6:00 AM Breakfast @ Gooderham Community Centre
7:00-9:00 AM Sign-in and sound test (at **Sedgwick's Farm**)
9:00 AM Start (at **Sedgwick's Farm**)
10:28 AM Gas Stop #1
10:45 AM Tunnel of Love Spectator Point
12:12 PM Tekvest Spectator Point
12:39 PM Gas Stop #2 & MX Test Spectator Point, Sedgewick's
1:16 PM Burnt river crossing spectator point
3:00 PM Final MX Test - 4 Lap motos for Pro, Expert and Vet A
4:00- 5:00 PM Vintage Bike show judging (at Gooderham Community Centre)
5:00 PM Dinner (at Gooderham Community Centre)
6:00 PM All Motorcycles must be in the impound
7:00 PM Awards for Vintage Show and the Trail Cutter Award
8:00 PM Cord videos
9:00 PM Protest period for Friday and Saturdays results closes

Sunday

6:00 AM Breakfast (at Gooderham Community Centre)
7:30-8:00 AM Sign-in for anyone who is riding only Sunday
8:15 AM First riders enter impound to bring their motorcycles to the work area
8:30 AM Start
8:31 AM Enduro X Test
8:50 AM Green's mountain Spectator Point
9:03 AM Gas Stop #1
9:50 AM Gas Stop #2
10:50 AM Extreme Test Spectator Point
12:30 PM Gas Stop #3
12:45 PM Green's Mountain Spectator Point
1:00 PM Finish and Final Enduro X Test
3:00 PM Results and Awards

IMPOUND RULES

1. No starting bikes in the impound area
2. Must push bike in and out of the impound area
3. No working on bikes in the impound area
4. No covers or tarps allowed on bikes
5. No touching another rider's bike
6. Must be in the impound by 6:00 PM
7. Riders will be allowed into the impound 15 minutes before their start times to get their bikes and bring them to the work area
8. No starting bikes in the work area
9. No starting bikes until the start signal is given
10. No one but the rider works on the bike. Support crew may put gas and oil in the bike and may also put air in the tires. The Women's and Novice C class riders are exempt from this rule and can have their designated mechanic assist them with repairs
11. Remember to turn off your gas tap when you put your bike in the impound
12. See supplementary rules for penalties regarding the above rules

Have a great ride!

BEMC & Haliburton Trail Riders

EXCERPTS FROM THE SUPPLEMENTARY RULES

4.0 TIMING AND SCORING

4.0.1 TEST SECTIONS

All test sections will be timed to the second using elapsed time and not based on a speed average. Timing will be done electronically with a manual backup. The backup system will utilize a score card attached to the front fender of the motorcycle where the rider's times will be recorded. The electronic timing is the official time. If there is a failure of the electronic system for a test section, times will be recorded on a back up sheet and the times on the score cards are official time.

4.0.2 CHECK POINTS

Check points, also known as take out points, will be located at various points along the route. Every gas stop will be a take out point in addition to other locations the clerk of the course deems necessary. The purpose of these check points is to determine if a rider is too far behind schedule to continue and also to ensure riders do not get too far ahead of schedule that the timing crews are not in place at the special tests.

4.0.2.1 CHECK POINT OPERATION

Every check point will be equipped with a set of flip cards which will display the row number of the riders that are due each minute based on the posted speed average. If a rider is more than the prescribed maximum limit behind schedule at any of the take out points they will be classified as DNF. Riders are not penalized for early or late arrival at check points but will be penalized if they proceed through check points ahead of their original due time. Two minutes for every minute ahead of original due time will be added to the riders score.

Example: Rider # 232 checks through check point # 3 when number 30 is showing on the flip cards. Rider # 232 will have 4 minutes added to their score.

4.0.2.3 CHECK POINT DNF TIME LIMITS

Friday, Day 1: 1 hour limit at all checkpoints

Saturday, Day 2 and Sunday, Day 3: 1 hour limit at all checkpoints with the exception of the final point at the end of the day where there will be a 2 hour limit.

Pro class will have only a 1 hour limit at the final check point on both Day 2 and Day 3.

4.0.2.4 PROCEDURE AFTER DNFing

Riders who DNF at one of take out points will not ride back to the start unless they are plated, licensed and insured to operate on public roads in Ontario. In addition the rider must have removed the Corduroy number plate sticker from his or her machine. All other riders who DNF will have to trailer their bikes to get back to the Parc Fermé/Start area. Most checkpoints will be located at the Gas Stops and the event volunteers will work with the riders to get all riders and machines back to the start area when a rider DNF's at a non-gas stop take out point.

4.0.2.5 TIME SCHEDULE

There are two time possible time schedules. Schedule A will be calculated using a speed average of 30 kph and schedule B which is calculated based on a 24 kph speed average. The clerk of the course will decide on which schedule is being used and have it posted at the start area each morning of the event. B schedule will be used if there is inclement weather.

4.0.2.6 SPEEDING PENALTIES

Speed limits will be enforced on transit section roads and rail beds. Speed limit on these sections will be 40 kph unless posted otherwise. Course marshals will use radar guns to calculate the following penalties:

11 kph over the limit - 1 min added to rider's score

15 kph over the limit - 5 min added to rider's score

20 kph over the limit - Disqualified

Not stopping for a course marshal or attempting to obscure rider number - Disqualified

No appeals or protests of a speeding penalty will be considered.

4.1 PARC FERME

Motorcycles will be impounded on Friday and Saturday nights immediately following the last checkpoint. Riders who do not wish to impound their motorcycles shall be considered DNF for event awards, prizes, prize money and finisher's medals. Due to the fact that the Corduroy Enduro is two points paying rounds (3 rounds for the Pro class) of the Off-road Ontario championships and the Canadian National Enduro championship, a rider who does not impound his/her motorcycle on Saturday night is still eligible for points towards the Off-road Ontario championships and the Canadian National Enduro championship. A rider who does not impound his/her motorcycle on Saturday night (Friday also for the Pro, Exp and Vet A class) and plans on riding day two must inform the Clerk of the Course or Jury President before the start of the next day. Failure to notify these officials shall result in disqualification from the results of all days and no points being awarded.

4.1.1 SECURITY

The Parc Fermé must be enclosed and fenced in to prevent unauthorized persons from entering. Its limits must be clearly marked out and supervised by a sufficient number of officials to ensure that only authorized persons may enter or have access to the machines. It must have one clearly marked entrance and one exit which leads to the "starting area", but no other entrances or exits. Officials in charge of the closed-controls must wear a distinctive emblem recognized by all persons concerned and the riders in particular. The Parc Fermé will be protected by a security guard from 7 pm Saturday until 7 am Sunday.

4.1.2 ACCESS

Access to any Parc Fermé is forbidden to everyone except the Jury members, certain officials designated for duty, and riders who are parking or taking out their motorcycles. After checking in at the time check just before the Parc Fermé, the rider must stop the engine and push his motorcycle into the Parc Fermé without delay. While proceeding from the time check to the Parc Fermé, it is forbidden to refuel or make any repairs on the motorcycle. Riders failing to comply with either of these requirements will be disqualified or other penalties given as provided for by the Jury.

4.1.3 PROHIBITED ACTIONS

In the Parc Fermé, it is forbidden for a rider, under penalty of disqualification (or other penalties as provided for by the jury) from the event:

- To touch the machine of any other rider;
- To touch his own machine except to push it in or out of the Parc Fermé;
- To start the engine.

4.1.4 SMOKING

Any rider caught smoking in a Parc Fermé will be disqualified.

4.1.5 COVERS

The machines in the Parc Fermé must not be covered in any manner.

4.2 WORK AREA

A work area is a small enclosed area where the riders await the starting signal and where the starting line is situated at one extremity. Pro/ Exp/ Vet A riders will be allowed into the impound area to get their motorcycles 15 minutes before their respective start time. Working on the motorcycle in the impound area is not permitted. Working on the motorcycle in work area is permitted until the starting signal is given. Any rider caught smoking in the work area will be disqualified.

4.2.1 ACCESS TO WORK AREA

Only Jury members, representatives and officials designated in the Supplementary Regulations, and the riders waiting for the start, are permitted access to the work area. The organizer will decide whether representatives of the press are permitted access to the work area and will specify the number allowed and under what conditions.

Each rider is allowed one mechanic in the work area. No other persons are allowed into the work area.

4.2.2 ASSISTANCE

No assistance may be given in the Work Area, with the exception of a rider's designated mechanic. While in the Work Area the designated mechanic is not allowed to do any repairs, maintenance, or touch the motorcycle except to replenish fluids, (gas, oil, coolant and brake fluid), inflate tires and check tire pressure. The penalty for breaking this rule is disqualification (or other penalties given as provided for by the Jury).

Women A&B and Novice C class riders are exempt from the restrictions stated in the preceding paragraph. The designated mechanics for riders in these classes may assist with any and all repairs. All repairs must be carried out in the work area and only the designated mechanic may assist.

SUNDAY Gas and Spectator Sheet

TEST #1 KEY TIME 08:48 LOCATION GREEN MOUNTAIN N44 51.983 W078 24.212

00.00 Kms Turn left onto Highway #507
and drive south to Green Mountain Road.
04.00 Kms Turn right onto Green Mountain
Trail.
05.50 Kms Trail intersection, big hill to the
left beside steep downward trail.
In all, you will have traveled 05.50 Kms to
your destination.
Red Plated Riders Only

GAS #1 and #3 09:02....12:03 LOCATION ON THE SALMON LAKE ROAD

NOTE: Two Separate Locations!!!!!!

GAS #1 N44 49.858 W078.24.409 GAS #3 N44
49.856 W078 25.113
00.00 Kms Turn left and drive south on Highway #
507,
09.50 Kms Turn right onto the Salmon Lake Road.
02.30 Kms Alf Von Zuben sign on left at Fire #459
just prior to gas stop #1.
00.10 Kms Gas Stop #1.
01.00 Kms Gas Stop #3.
In all, you will have traveled 12.90 Kms to your
furthest destination.

Gas #1

Salmon Lake Rd

Gas #3

Gas #2

Time 10:14 NEW LOCATION!!

Exit Rec Center, and turn left on HWY
507, go South 28.8 km to Fire
Access Rd 1, turn right. Go 6.3 km's,
do not turn left go Straight ahead,
travel 7 more km's to gas stop #2. If
you reach the Quarry you have
missed the straight ahead bit....

Gas #2

752-800 Fire Access Rd

1 Fire Access Rd

h

v

Saturday Gas and Spectator Sheet

**Return to Start
Finish Area at
Sedgewicks**

**Return to Gooderham
Rec Center**

TO FINAL MX TEST FROM GAS # 1

00.00 From Gas Stop #2 on the Pinery Road, drive north on the Pinery Road.

2.7 Kms Stop turn right onto County Road #45, the Monck Road.

04.20 Kms Stop turn left onto Highway #121 and drive north.

07.50 Kms Turn right onto County Road #1 driving east.

09.90 Kms Turn right onto the Sedgewick's Road and drive south.

00.80 Kms Turn right onto Francis Road. (second road on your right)

00.40 Kms Turn left onto the Rail Trail.

00.40 Kms Turn right into the gates of the Rail Trail Pit.

In all, you will have traveled 25.9 Kms.

 23 min
15.9 km

PARKING

**Spectator Point: Tunnel of Love and
Sommerville Forest. 11:05**

Continue North on Pinery Rd to just before Intersection with Monck Rd, park in parking lot on right. Feel free to walk through the forest to the Tunnel Of Love.

Gas # 1 10:28

From Start Area, return to Hwy #121, Turn left on HWY 121, travel 11.6 km through Kinmount, then turn right on Conc 11, go 3.8 km to Pinery Rd, turn right and park on side of road.

Concession Road 11

Concession Road 11

Walsten Marine

Google

